

PANEL ELEVATION

POST ELEVATION

POST DETAILS

PANEL TYPE	PANEL LAYOUT	OPTIMUM SIGHT ANGLE	TYPICAL USES
V2		2.5° TO 5°	STRAIGHT ROADS, MODERATE TO HIGH SPEEDS
V4		5° TO 14°	STRAIGHT ROADS, LOW TO MODERATE SPEEDS CURVES
V8		MORE THAN 14°	CURVES LESS THAN 15m RADIUS, E.G. BELLMOUTHS

NOTES:

1. PEDESTRIAN GUARDRAILS SHALL CONFORM TO THE REQUIREMENTS OF BS7818:1995 AND BE ERECTED IN ACCORDANCE WITH RECOMMENDATION OF THE MANUFACTURER.
2. STANDARD FINISH HOT DIP GALVANISED TO BS EN 1461.
3. ALL DIMENSIONS ARE IN MILLIMETRES.
4. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.

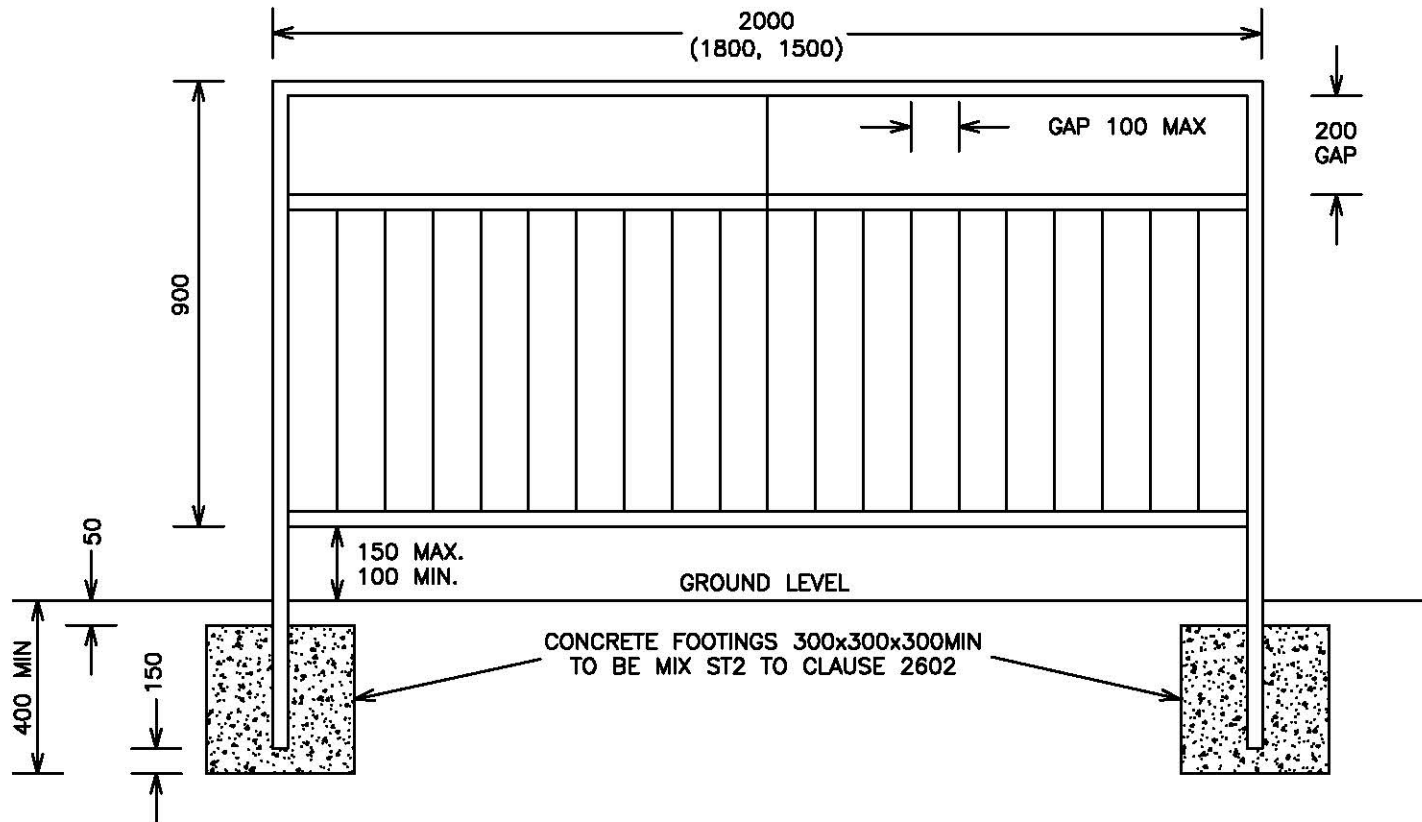


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DRAWING TITLE
 PEDESTRIAN GUARDRAIL
 'VISIRAIL' TYPE PEDESTRIAN GUARDRAIL

REV.	DESCRIPTION	CHECKED	DATE

SURVEYED BY	INIT.	DATE	PROJECT TITLE	
			DEVELOPER STREET	
DESIGNED BY			SCALE	DATE
			N.T.S	APRIL 15
DRAWN BY			DRAWING No.	
			DSD 400-01	
CHECKED BY				



NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. PANELS TO BE STEEL CONSTRUCTION. FINISH TO BE HOT DIP GALVANISED TO BS EN 1461 UNLESS SPECIFIED OTHERWISE.
3. INFILL TO BE VERTICAL BARS (NUMBER OF INFILL BARS TO BE DETERMINED BY THE MANUFACTURER).
4. FRAME TO BE HOT ROLLED STEEL HOLLOW SECTION.
5. LENGTH OF END PANEL TO BE AS SPECIFIED.
6. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.



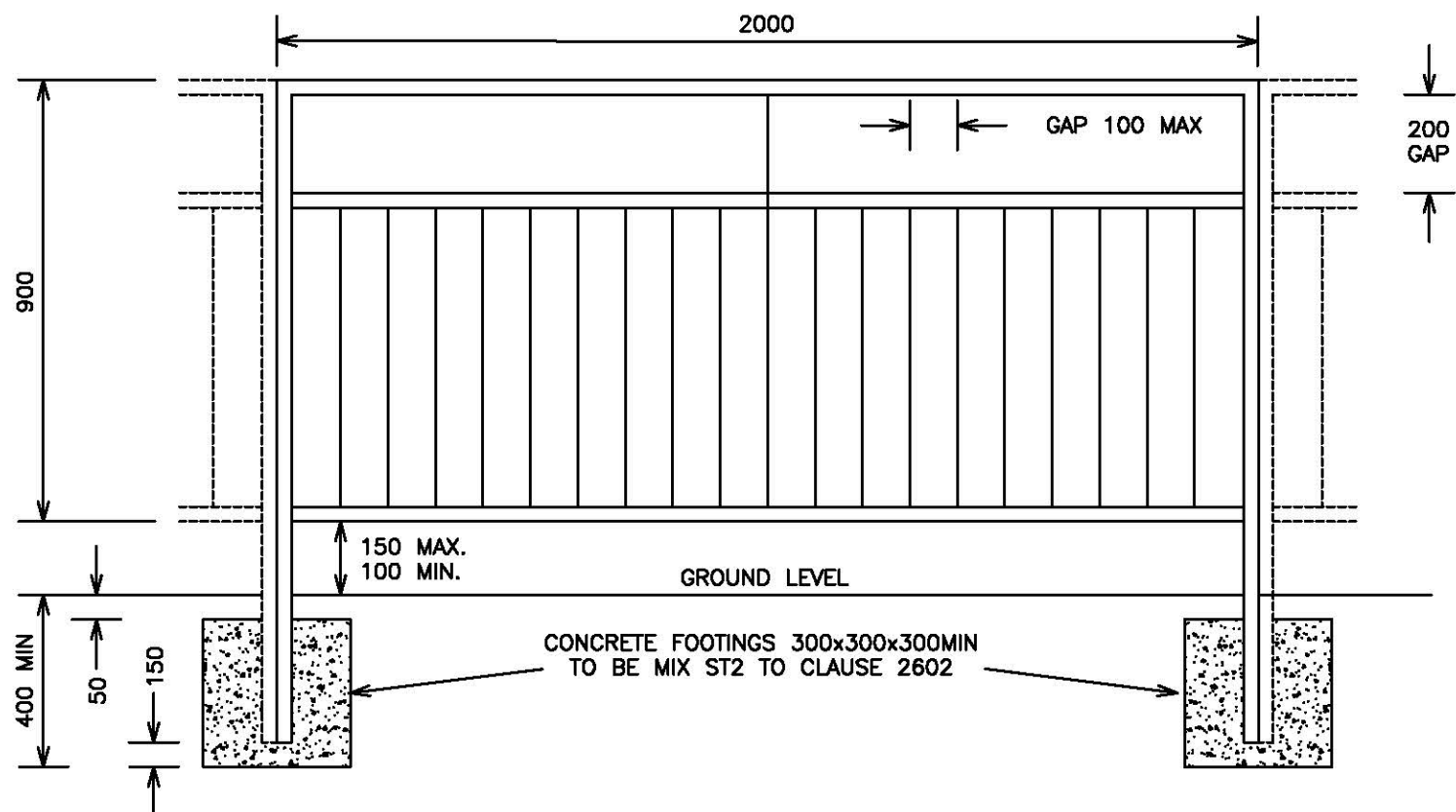
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DRAWING TITLE

**PEDESTRIAN GUARDRAIL
 STANDARD PEDESTRIAN GUARDRAIL
 (END PANEL)**

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		DEVELOPER STREET SCAPE DETAIL	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	APRIL 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-400-02	



NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. PANELS TO BE STEEL CONSTRUCTION. FINISH TO BE HOT DIP GALVANISED TO BS EN 1461 UNLESS SPECIFIED OTHERWISE.
3. INFILL TO BE VERTICAL BARS (NUMBER OF INFILL BARS TO BE DETERMINED BY THE MANUFACTURER).
4. FRAME TO BE HOT ROLLED STEEL HOLLOW SECTION.
5. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.



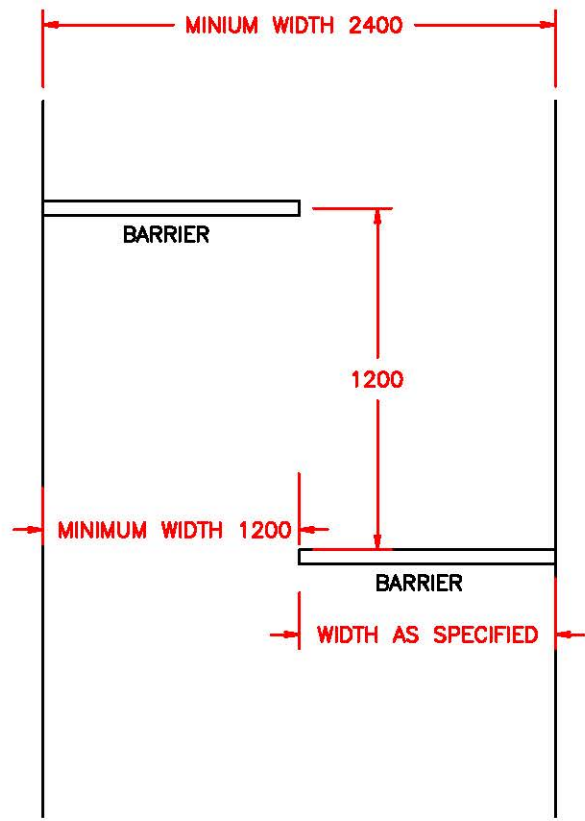
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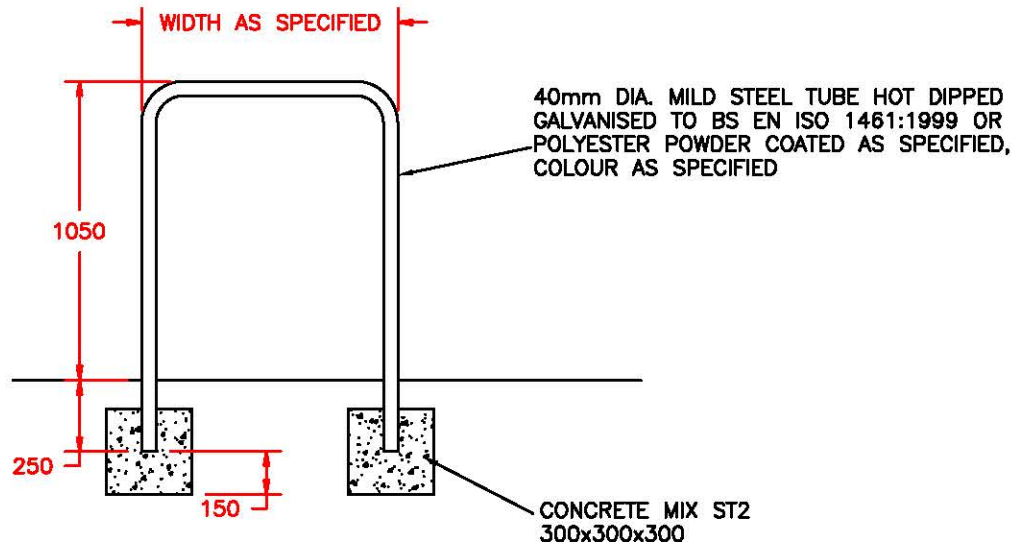
**PEDESTRIAN GUARDRAIL
 STANDARD PEDESTRIAN GUARDRAIL
 (INTERMEDIATE PANEL)**

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		DEVELOPER STREET SCAPE DETAIL	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	APRIL 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-400-03	



PLAN



ELEVATION ON BARRIER

NOTES:

1. ALL DIMENSIONS IN MILLIMETRES.
2. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.
3. STAGGERED BARRIERS SHOULD NOT TO BE USED BELOW 2.4M FOOTWAY WIDTH. IN THESE INSTANCES ALTERNATIVE METHODS TO DETER INAPPROPRIATE VEHICLE USE SHOULD BE EXPLORED EG. BOLLARDS

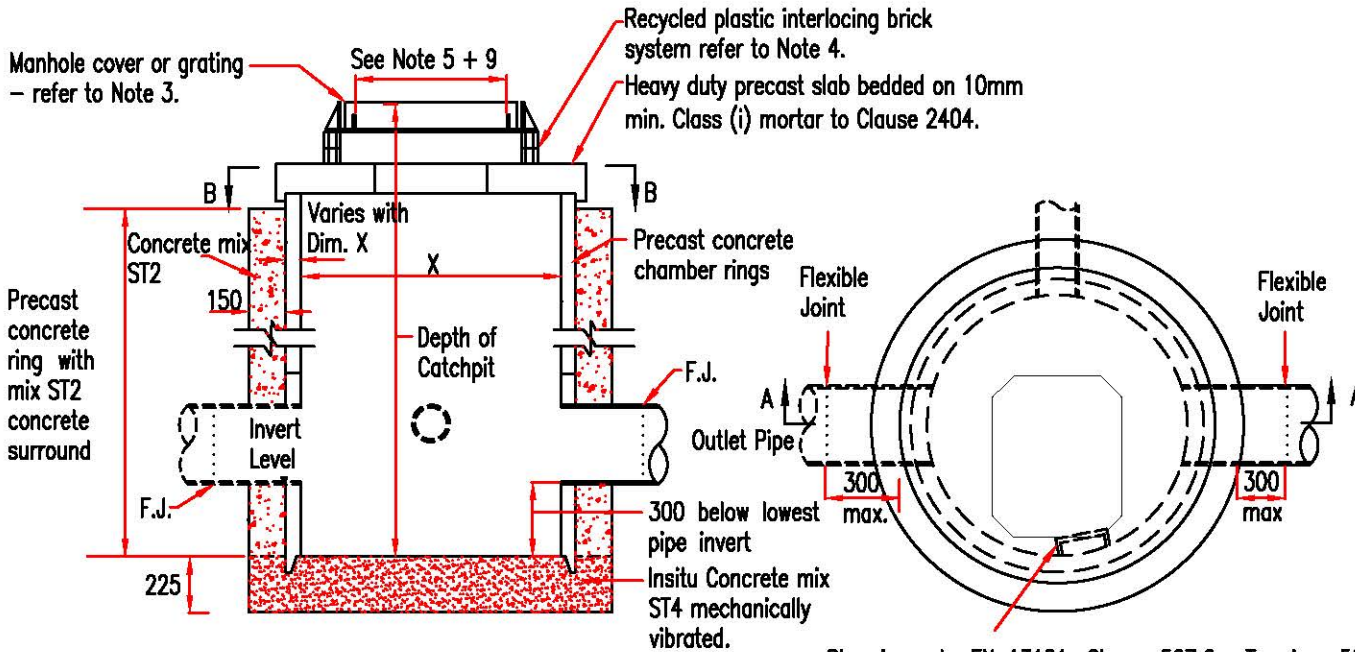


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DRAWING TITLE
 PEDESTRIAN GUARDRAIL
 STAGGERED SAFETY BARRIER

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Manhole cover or grating
– refer to Note 3.

See Note 5 + 9

Recycled plastic interlocking brick system refer to Note 4.
Heavy duty precast slab bedded on 10mm min. Class (i) mortar to Clause 2404.

Precast concrete ring with mix ST2 concrete surround

Concrete mix ST2

150

Varies with Dim. X

X

Depth of Catchpit

Invert Level

225

300 below lowest pipe invert
Insitu Concrete mix ST4 mechanically vibrated.

Step irons to EN 13101, Clause 507.6. Top iron 500 max from cover level, bottom iron 300 above concrete base. Ladder to be used if depth to invert exceeds 3m, in accordance with HCD F10.

PLAN ON B-B

SECTION A-A

DIM. X VARIES TO 1050, 1200, 1500 or 1800 as specified in Appendix 5/1.

CHAMBER RING DIAMETER	MAX. PIPE DIAMETER
1050	375
1200	450
1500	700
1800	900

NOTES

- All dimensions in millimetres.
- Catchpit to be constructed in precast concrete to BS 5911:Part 200.
- Manhole cover or grating as specified in Appendix 5/1. D400 double triangular non-rock in carriageway. Covers and frames in areas accessible to pedestrians only shall be B125.
- Where covers are used within the carriageway, they shall be laid on a proprietary recycled plastic interlocking brick system. Where covers are used in verges and footway Class B engineering bricks may be used as an alternative.
- Heavy duty precast cover slab placed with opening away from traffic wherever possible in verge and nearest to channel in carriageway.
- Where catchpits are constructed in the carriageway the top of the slab will be at formation level and the depth between the slab and frame will be constructed in plastic interlocking brick system.
- Taper and shaft rings may be used provided bottom of taper is a minimum of 1.8m above concrete base.
- Number of pipe openings to be as detailed in Appendix 5/1 and/or as shown on construction drawing.
- All carrier pipes shall have the same soffit level unless detailed otherwise in Appendix 5/1.
- Access opening to chambers as DCD-0500-06.
- Pipes to have flexible joints (F.J.) not greater than 600mm from chamber wall.
- Where covers are being reset due to failure, plastic interlocking bricks are to be used. Reinstatement to be proprietary mastic asphalt where appropriate, if not HRA.



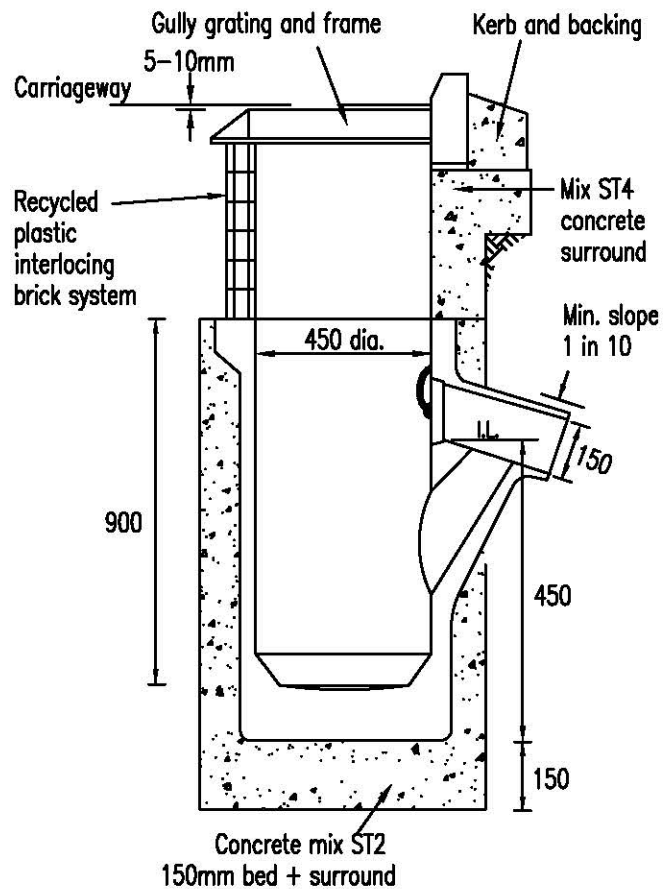
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DRAWING TITLE

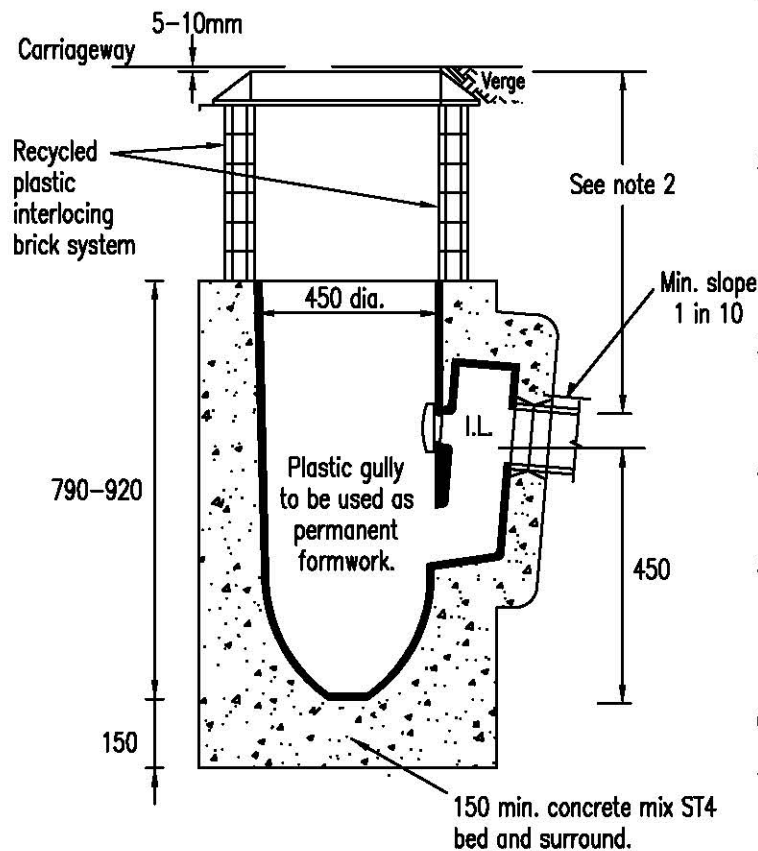
DRAINAGE AND SERVICE DUCTS
PRECAST CONCRETE RING CATCHPIT

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INIT.	DATE	PROJECT TITLE	
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PRECAST UNIT SHOWN WITH KERB



INSITU GULLY SHOWN WITH VERGE

NOTES

Gully

1. Gully to comply with Specification Clause 508 and may be cast insitu using a plastic gully as permanent formwork, or precast concrete to B.S 5911.
2. The minimum depth from the top of the grating to the top of the gully outlet is to be 750 when the connecting pipe is under the carriageway, elsewhere 600.

Grating and Frame

3. Gully grating and frame to B.S EN124. To be D400 double triangular or hinged as required by the overseeing organisation.
4. Gully frame to be set 5 – 10mm below carriageway surface.
5. Where covers are being reset due to failure, plastic interlocking bricks are to be used. Reinstatement to be proprietary mastic asphalt where appropriate, if not HRA.
6. All dimensions in millimetres.
7. Both precast and insitu gullies can be installed adjacent to kerb or verge.



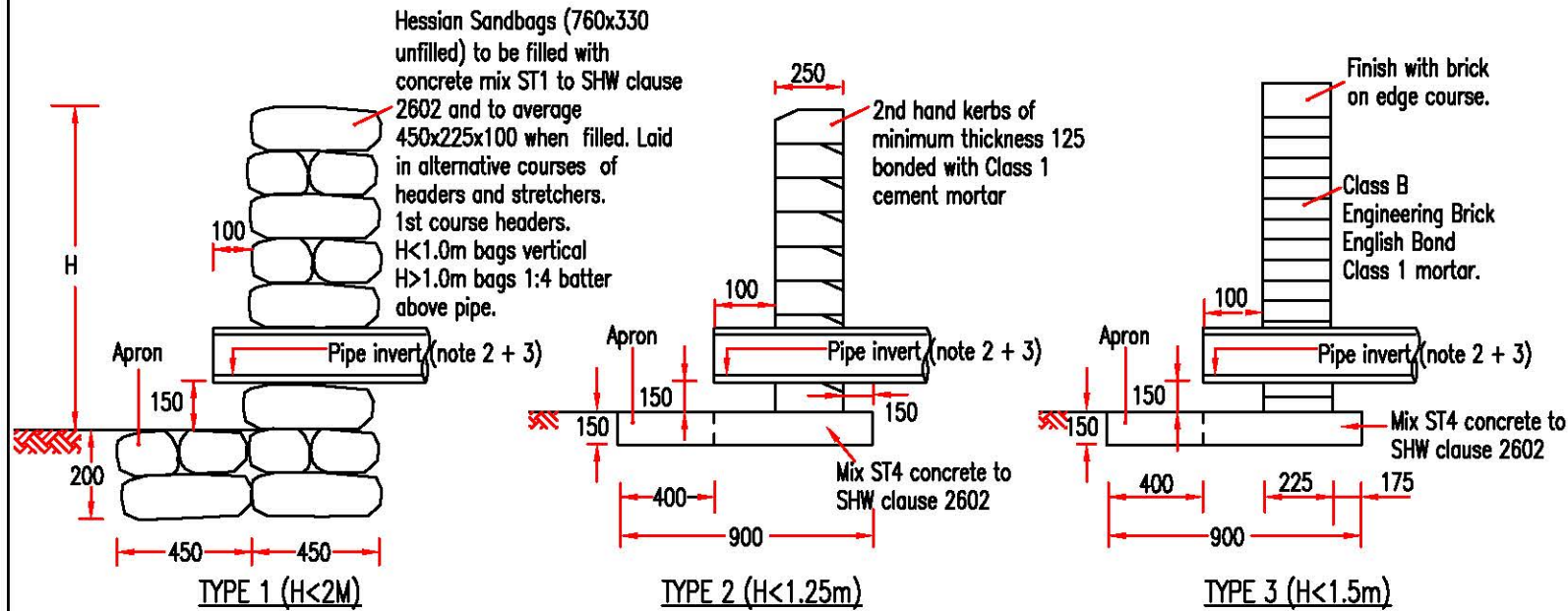
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DRAWING TITLE

**DRAINAGE AND SERVICE DUCTS
 PRECAST AND INSITU CAST GULLIES**

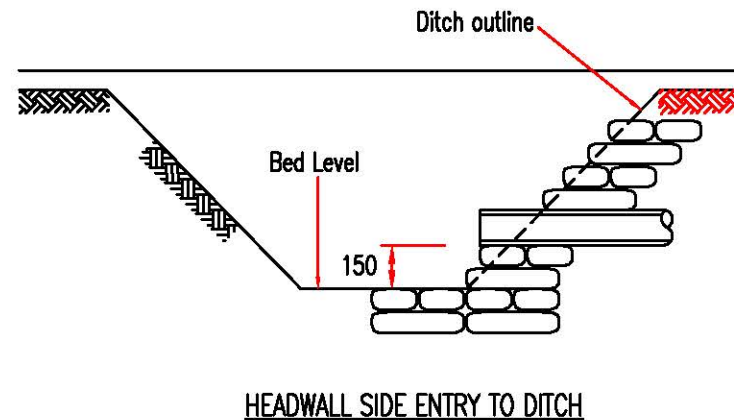
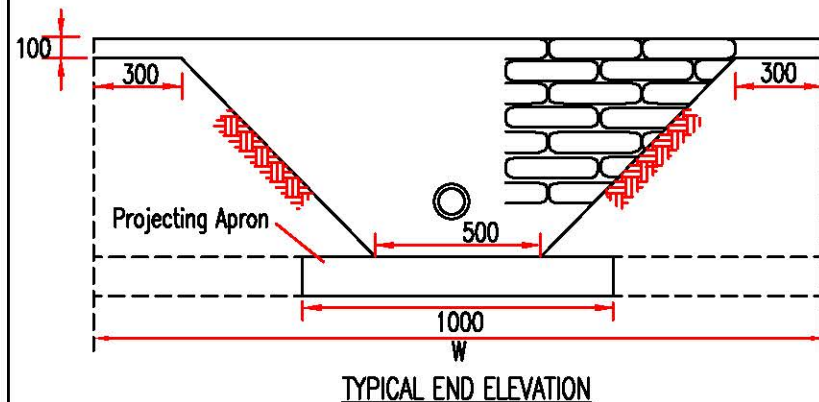
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CHECKED BY		DRAWING No. DSD-0500-03	



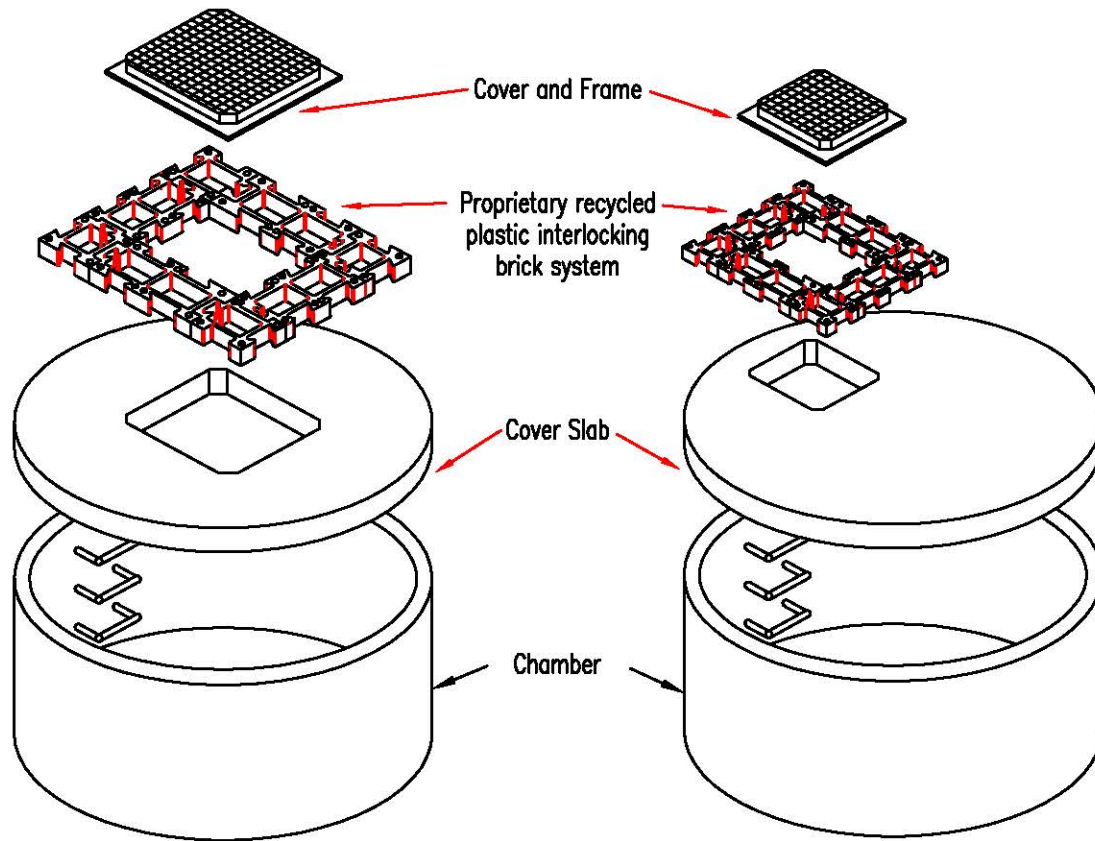
NOTES

1. Dimensions H and W as specified on Construction Drawing. Side slopes to ditches either as existing or as specified otherwise on Construction Drawing.
2. The first pipe joint shall be flexible and located not more than 300mm from the back of the headwall.
3. Where the ditch outfalls into the pipe the invert of the pipe shall be level with the top of the apron.
4. All dimensions in millimetres unless otherwise stated.



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CENTRAL

ECCENTRIC

Shaft/chamber size (Int. Dia)	Position and size of access	
	Manhole less than 1.5m depth (cover level to pipe soffit – mm)	Manhole greater than or equal to 1.5m depth (cover level to pipe soffit – mm)
1050	750 x 750 central	600 x 600 eccentric
1200	750 x 600 eccentric*	750 x 600 eccentric
1500	1200 x 675 central	750 x 600 eccentric
1800	1200 x 675 eccentric	750 x 600 eccentric
2100	1200 x 675 eccentric	750 x 600 eccentric
2400	1200 x 675 eccentric	750 x 600 eccentric
3000	1200 x 675 eccentric	750 x 600 eccentric
*(1200 x 675 permitted but not recommended)		



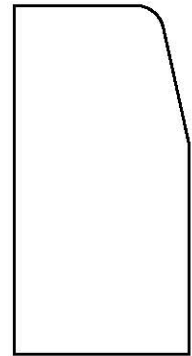
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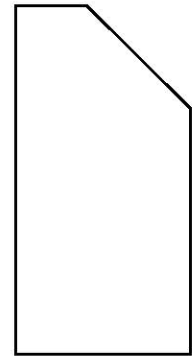
**Drainage and Service Ducts
 Access to Chambers**

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
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SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	April 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-0500-06	



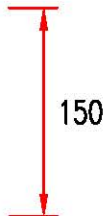
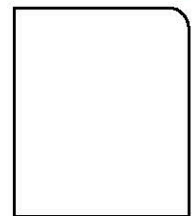
TYPE HALF BATTERED
KERB (HB2)



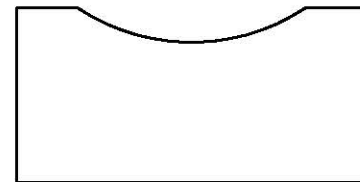
TYPE SPLAYED
KERB (SP)



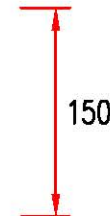
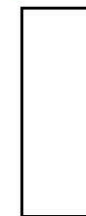
TYPE CHANNEL SQUARE
KERB (CS1)



TYPE BULLNOSED
KERB (BN)



TYPE CHANNEL
DISK (CD)



TYPE FLAT TOP
EDGING KERB (EF)

NOTES

1. All kerb type designations comply with BS EN 1340.
2. All dimensions are in millimetres.

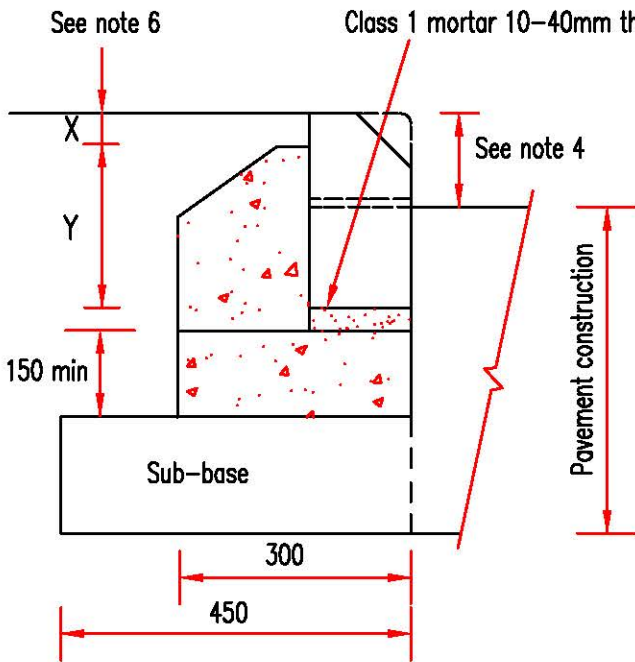


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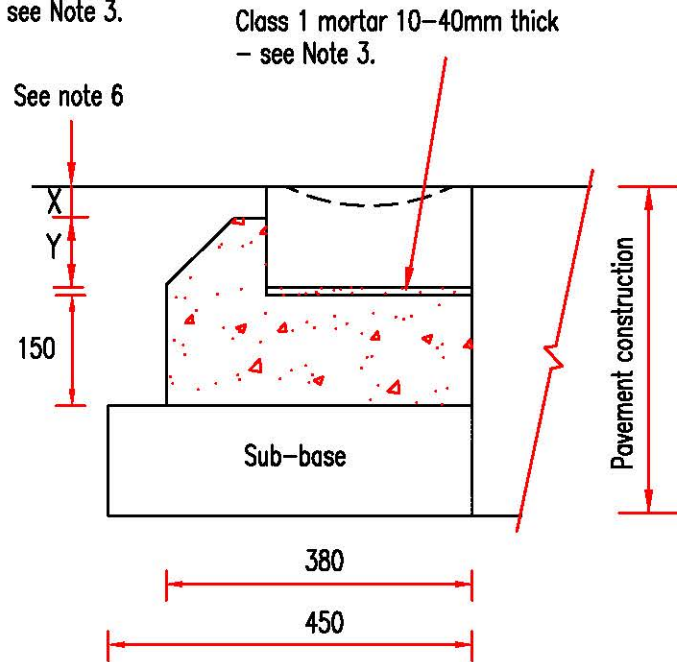
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KERBS, FOOTWAYS & PAVED AREAS
CONCRETE KERB TYPES
FOR HIGHWAYS & FOOTWAYS

REV.	DESCRIPTION	CHECKED	DATE

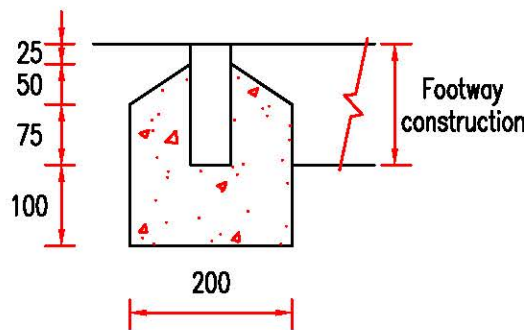
INIT.	DATE	PROJECT TITLE	
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Kerb Types HB, SP & BN



Channel Types CS & CD



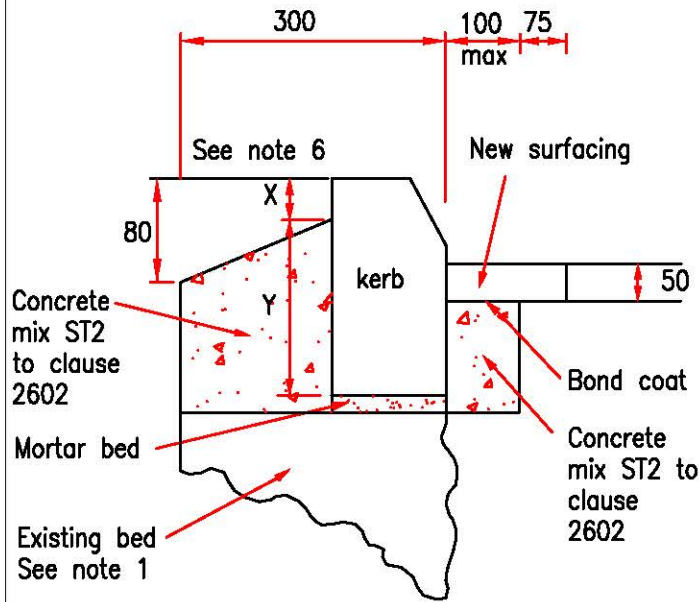
Flat Top Edging Type EF

Notes

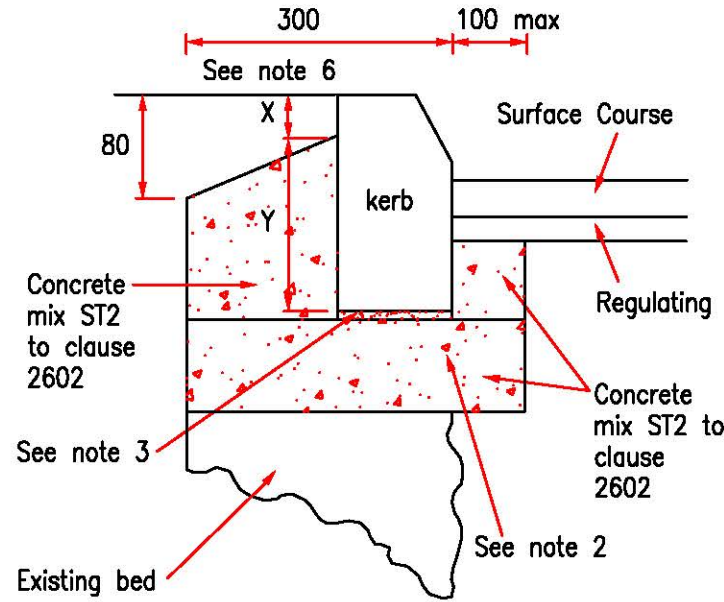
1. These details are for kerbing laid in conjunction with new carriageway construction including haunching.
2. Concrete bed to be mechanically vibrated and the surface roughened to provide a key for backing.
3. The mortar bed may be omitted if method of laying kerbs direct onto wet concrete is approved by Norfolk County Council. The backing concrete must be placed within one hour of laying the concrete bed. All concrete to be mix ST2 to SHW clause 2602.
4. Kerb face to be 125mm unless otherwise agreed with Norfolk County Council.
5. Kerbs laid to smooth horizontal and vertical alignment.
6. X dependant upon treatment behind kerb. For verge 40mm min. For footway/access = depth of surfacing. Y to be $\frac{2}{3}$ rds kerb depth minimum unless otherwise specified.
7. 0-6mm upstand for pedestrian crossings and 25mm upstand for vehicular access.
8. All dimenons in millimetres.

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No Carriageway Overlay



Overlay

Table of Bedding Materials

Bed Depth	Material
10 – 40mm	Class 1 mortar
25 – 75mm	Class 7.5/10 Concrete
50 – 150mm	Mix ST2 Concrete

NOTES

- Existing level to be mechanically reduced and roughened where required to provide new bed of 10mm minimum.
- Concrete bed to be mechanically vibrated and the surface roughened to provide a key for backing.
- Mortar bed may be omitted if method of laying kerbs direct onto wet concrete is approved by Norfolk county Council. The backing concrete must be placed within one hour of laying the concrete bed.
- Kerb face to be 125mm unless otherwise Agreed with the County Council.
- Kerbs laid to smooth horizontal and vertical alignment.
- X dependant upon treatment behind kerb. For verge 40mm Min. For footway/access = depth of surfacing. Y to be $\frac{2}{3}$ rds kerb depth minimum unless otherwise specified.
- Surfacing as specified in construction drawing.
- All dimensions are in mm.
- To be read in conjunction with DSD-1100-01.



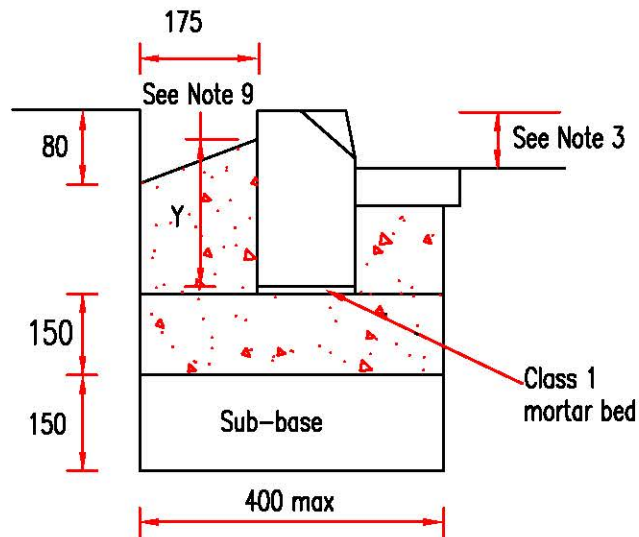
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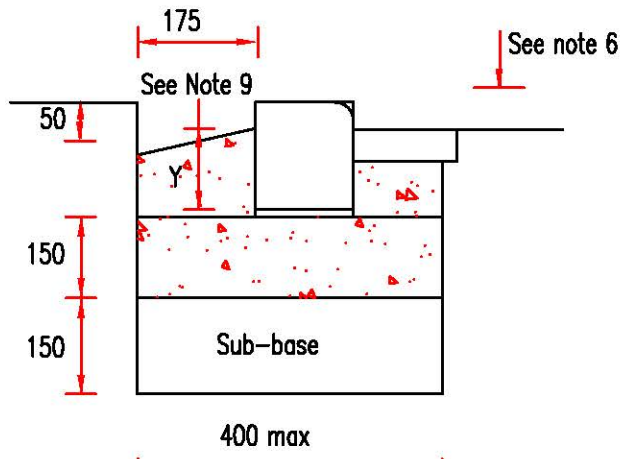
**KERBS, FOOTWAYS & PAVED AREAS
 KERBING LAID ON EXISTING BED**

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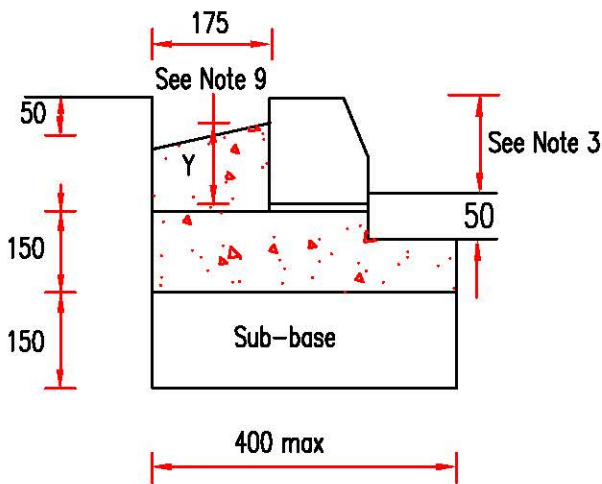
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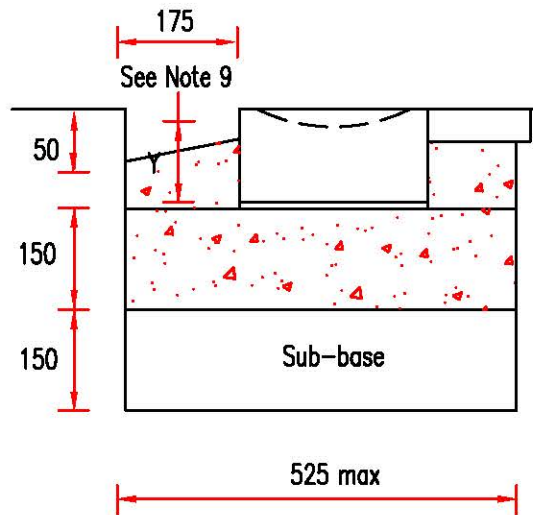
KERBS TYPES HB2 & SP



KERB TYPE BN



KERB TYPE HB3



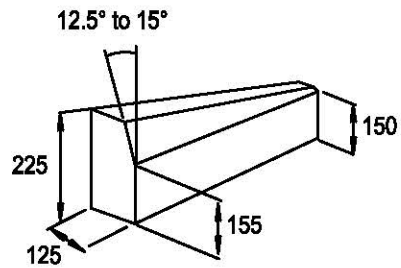
CHANNEL TYPES CS & CD

NOTES

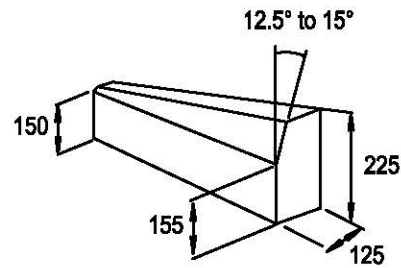
1. Concrete bed to be mechanically vibrated and the surface roughened to provide a key for backing.
2. The mortar bed may be omitted if method of laying kerbs direct onto wet concrete is approved by Norfolk County Council. The backing concrete must be placed within one hour of laying the concrete bed. all concrete to be mix ST2 to SHW clause 2602.
3. Kerb face to be 125mm unless otherwise agreed with the County Council
4. Surfacing as per construction drawing.
5. All dimensions in millimetres.
6. 0-6mm upstand for pedestrian crossings and 25mm upstand for vehicular access.
7. Making good at front of kerb to SHW Clause 706.
8. To be read in conjunction with DCD-1100-01.
9. Y to be $\frac{2}{3}$ rds kerb depth minimum unless otherwise specified.

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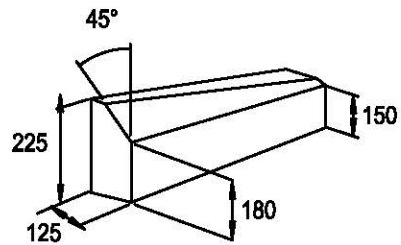
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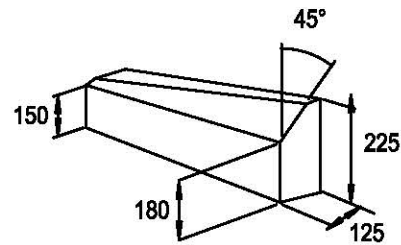
LEFT HAND TYPE DL1



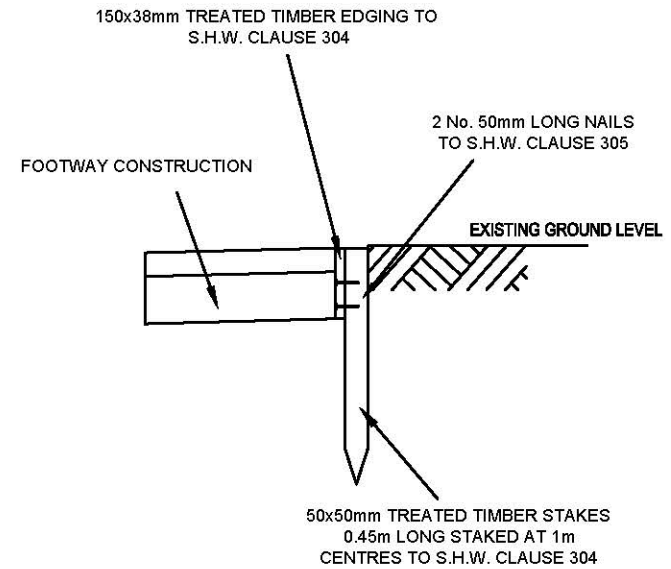
RIGHT HAND TYPE DR1



LEFT HAND TYPE DL2



RIGHT HAND TYPE DR2



NOTES:

1. PRE-CAST UNITS WEIGHING IN EXCESS OF 20kgs ARE TO BE PLACED BY MECHANICAL MEANS.
2. TIMBER TO BE TREATED IN ACCORDANCE WITH BS EN 351-1
3. NO MECHANICAL EXCAVATION WILL BE PERMITTED DUE TO PRESENCE OF TREE ROOTS.
4. IF DIRECTED, THE CONTRACTOR SHALL SUBSTITUTE TYPE 1 SUB-BASE MATERIAL WITH SAND WHERE THE FOOTWAY IS IN CLOSE PROXIMITY TO TREES.
5. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.



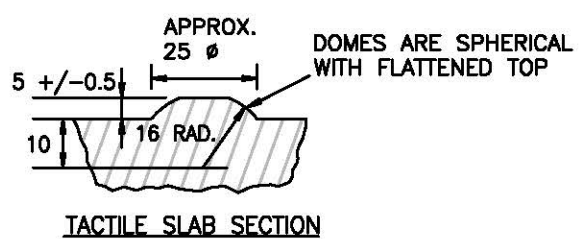
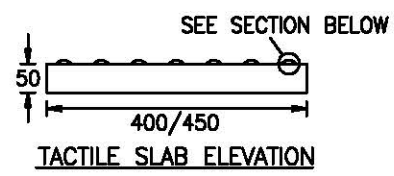
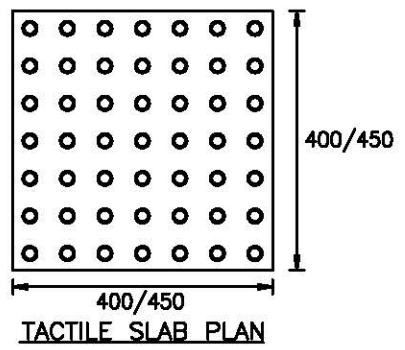
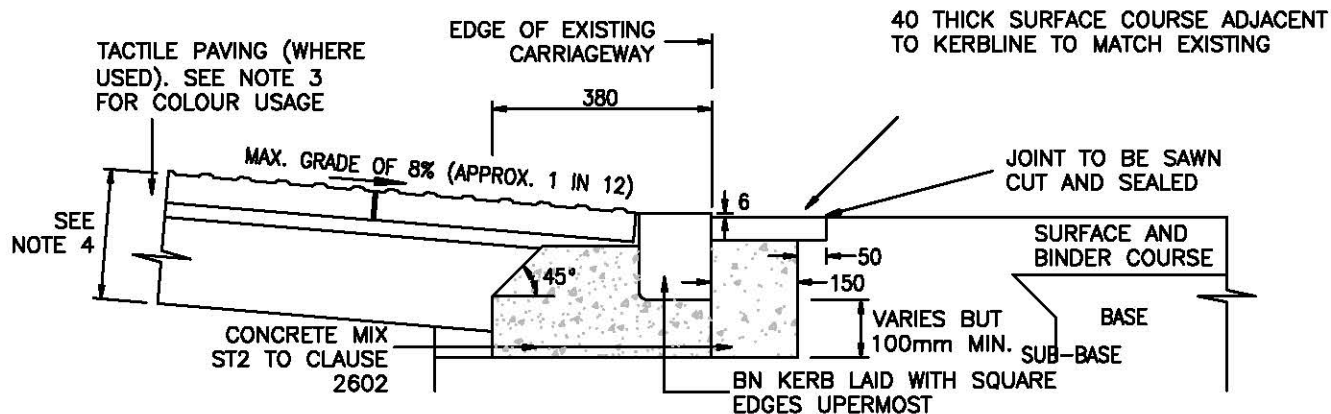
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DRAWING TITLE

**KERBS, FOOTWAYS & PAVED AREAS
 DL & DR KERBS, TIMBER EDGINGS**

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		DEVELOPER STREET SCAPE DETAIL	
		SCALE	DATE
		N.T.S	APRIL 2015
		DRAWING No.	
		DSD-1100-05	



KERB TYPE K4A

NOTES:

DETAIL WHERE KERB IS AT EXISTING EDGE

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED
2. TACTILE SLABS TO BE LAID PARALLEL TO THE CROSSING.
3. TACTILE SLABS TO BE COLOURED RED AT SIGNAL CONTROLLED CROSSINGS AND BUFF AT OTHER CROSSINGS.
4. TACTILE SLABS TO BE BEDDED ON 25mm THICK 3:1 CEMENT MORTAR ON 150mm THICK TYPE 1 SUB-BASE.
5. INSPECTION CHAMBER AND MANHOLE COVERS WITHIN AREA OF TACTILE PAVING TO BE FITTED WITH RECESSED COVERS INLAID WITH TACTILE PAVING.
6. PRE-CAST UNITS WEIGHING IN EXCESS OF 20kgs ARE TO BE PLACED BY MECHANICAL MEANS.
7. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.

DESIGN NOTES:

FOR GENERAL DETAILS OF TACTILE SLAB LAYOUT, REFER TO DRAWING No. DSD-1100-11 AND DEPARTMENT FOR TRANSPORT DOCUMENT: ["GUIDANCE ON THE USE OF TACTILE PAVING SURFACES"](#)

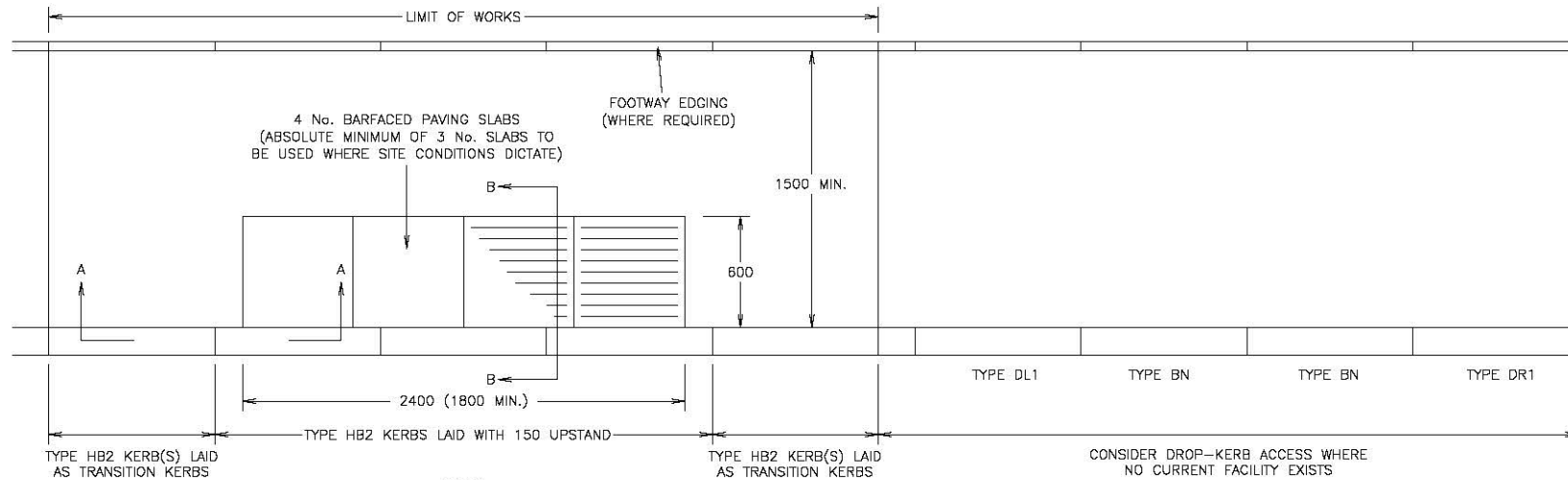
Norfolk County Council

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 County Hall
 Martineau Lane
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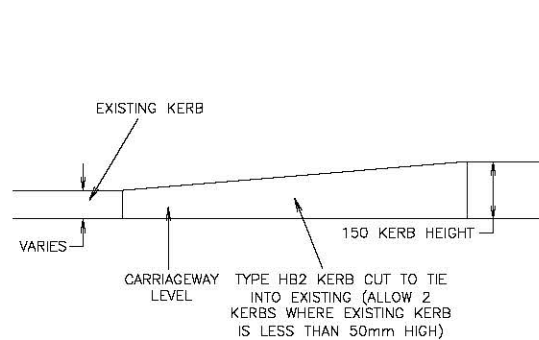
DRAWING TITLE
 KERBS, FOOTWAYS
 PEDESTRIAN AND CYCLE CROSSING POINTS
 TYPICAL DETAILS

REV.	DESCRIPTION	CHECKED	DATE

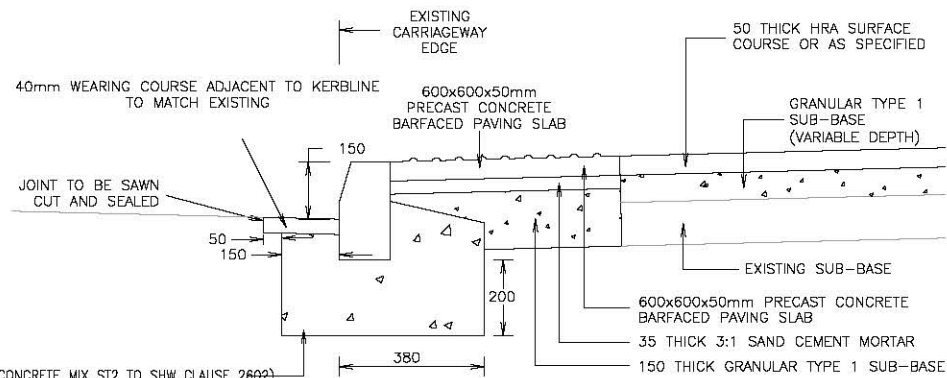
INIT.	DATE	PROJECT TITLE	
		STANDARD CONSTRUCTION DETAILS	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	APRIL 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1100-06	



PLAN



SECTION A-A



SECTION B-B

NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE INDICATED.
2. MAXIMUM FOOTWAY GRADIENT TO BE 8% (1:12 APPROX.).
3. PRE-CAST UNITS WEIGHING IN EXCESS OF 20kgs ARE TO BE PLACED BY MECHANICAL MEANS. IF BREAKING-OUT POSSIBLE.
4. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.



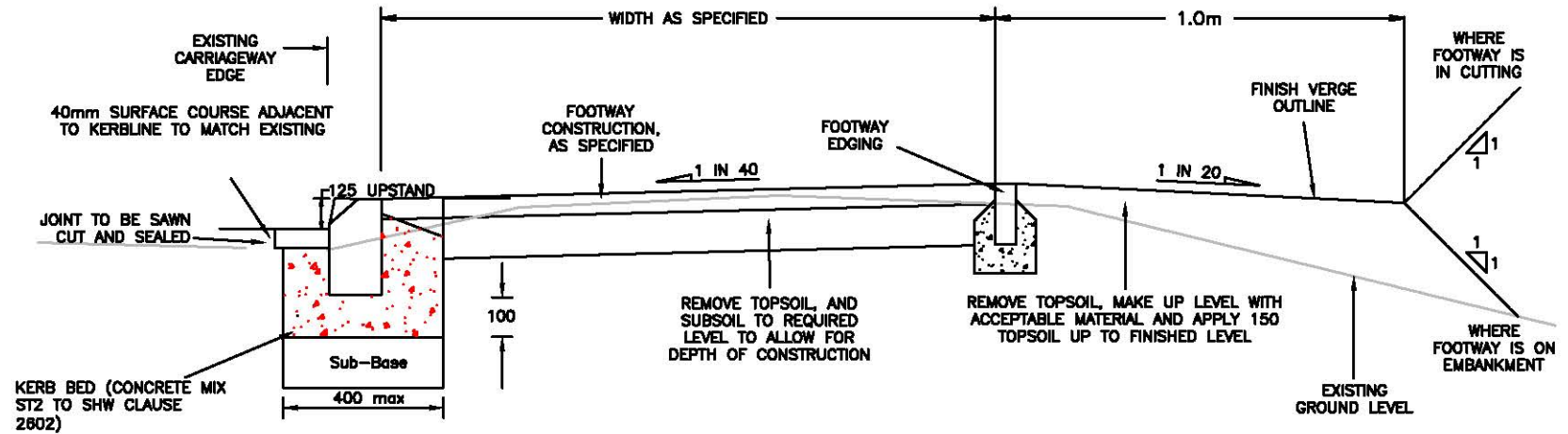
Tom McCabe
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DRAWING TITLE

**KERBS, FOOTWAYS
 BUS STOP KERB**

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		DEVELOPER STREET SCAPE DETAIL	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	APRIL 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1100-07	



NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE INDICATED.
2. PRE-CAST UNITS WEIGHING IN EXCESS OF 20kgs ARE TO BE PLACED BY MECHANICAL MEANS.
3. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.



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DRAWING TITLE
 KERBS, FOOTWAYS
 FOOTWAY TYPICAL CROSS SECTION

REV.	DESCRIPTION	CHECKED	DATE

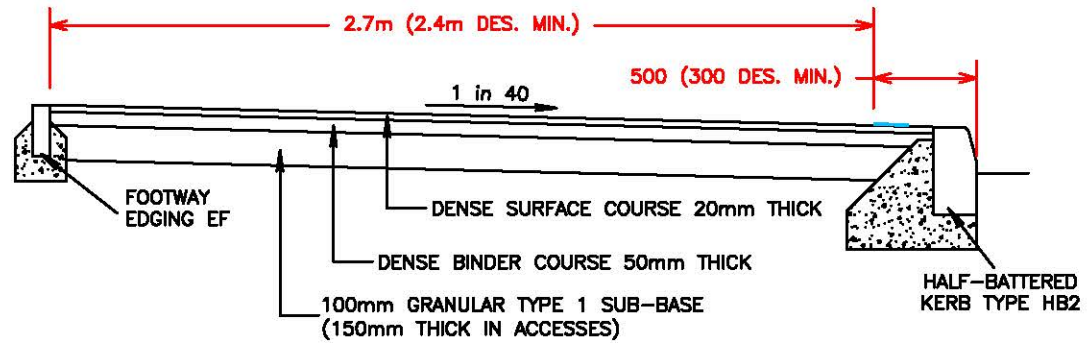
PROJECT TITLE			
DEVELOPER STREET			
SCAPE DETAIL			
SURVEYED BY	INIT.	DATE	
DESIGNED BY		SCALE	DATE
		N.T.S	MAY 15
DRAWN BY		DRAWING No.	
		DSD-1100-08	
CHECKED BY			

FOOTWAY/CYCLETRACK+EDGE STRIP WIDTHS

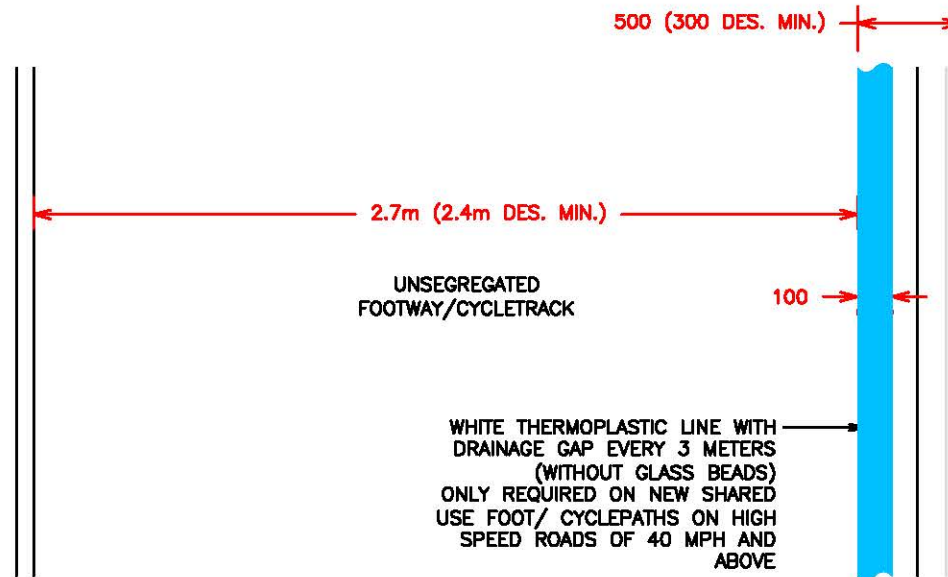
RECOMMENDED WIDTH (STANDARD)
3.2m including line if required

DESIRABLE MINIMUM WIDTH
2.7m including line if required

SHORT STRETCHES FOR ISOLATED OBSTRUCTIONS
1.8m including line if required



TYPICAL SECTION



PLAN

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
2. FOR FURTHER GUIDANCE ON CYCLETRACK WIDTHS, REFER TO SUSTRANS GUIDE 'THE NATIONAL CYCLE NETWORK - GUIDELINES AND PRACTICAL DETAILS'.

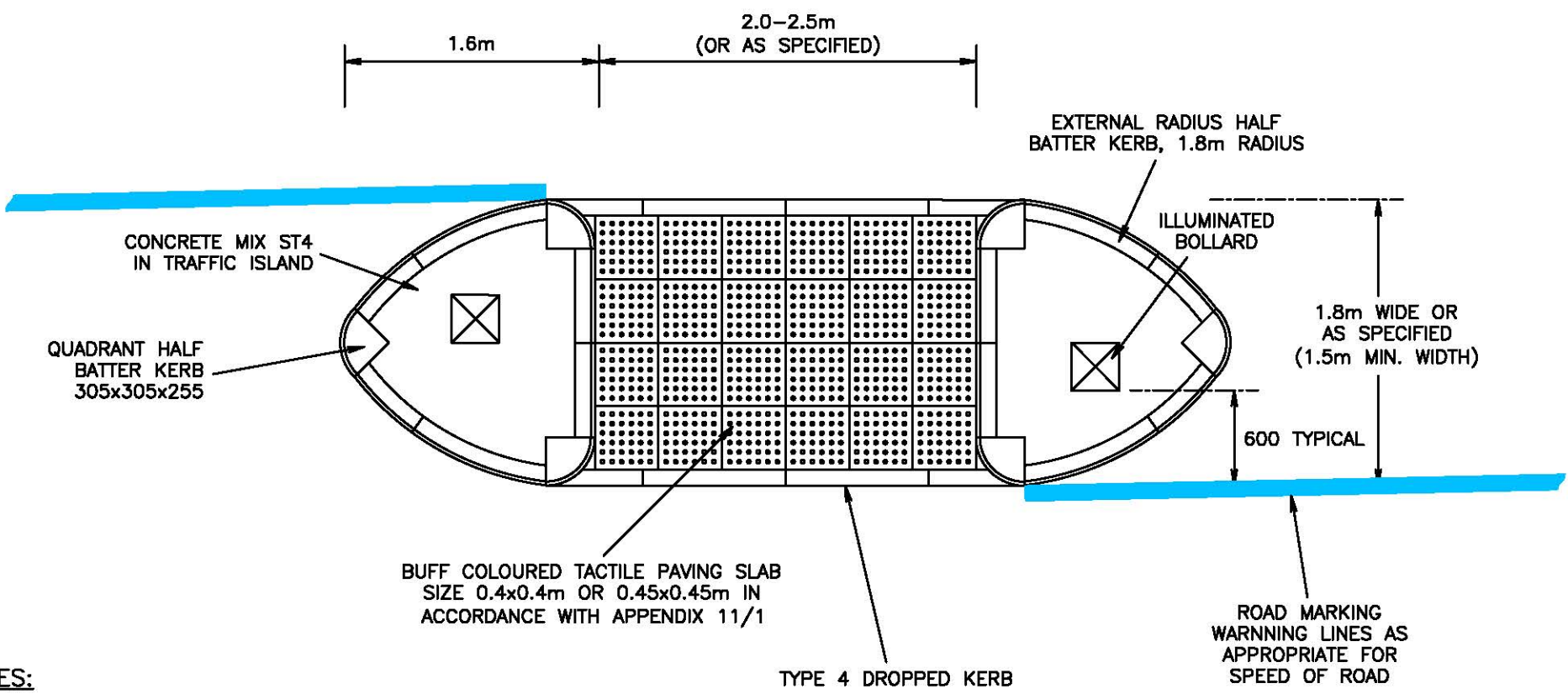


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DRAWING TITLE
KERBS, FOOTWAYS
CYCLETRACK ADJACENT TO CARRIAGEWAY

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		DEVELOPER STREET SCAPE DETAIL	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	APRIL 15
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1100-08	



NOTES:

1. PRE-CAST UNITS WEIGHING IN EXCESS OF 20kgs ARE TO BE PLACED BY MECHANICAL MEANS.
2. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.
3. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.

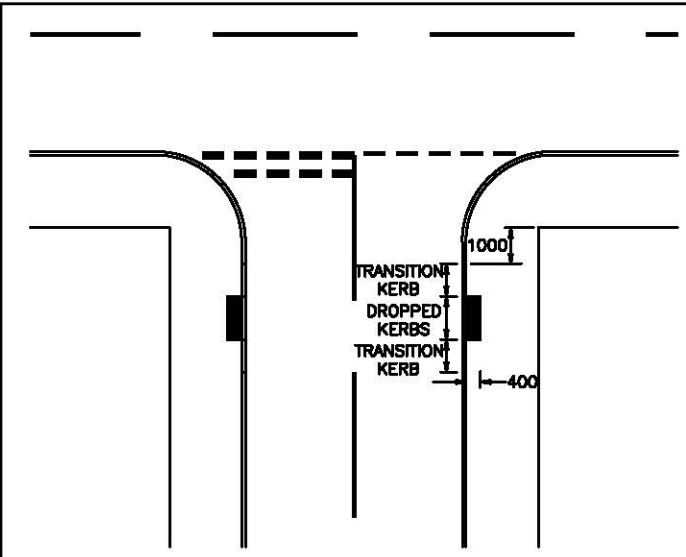


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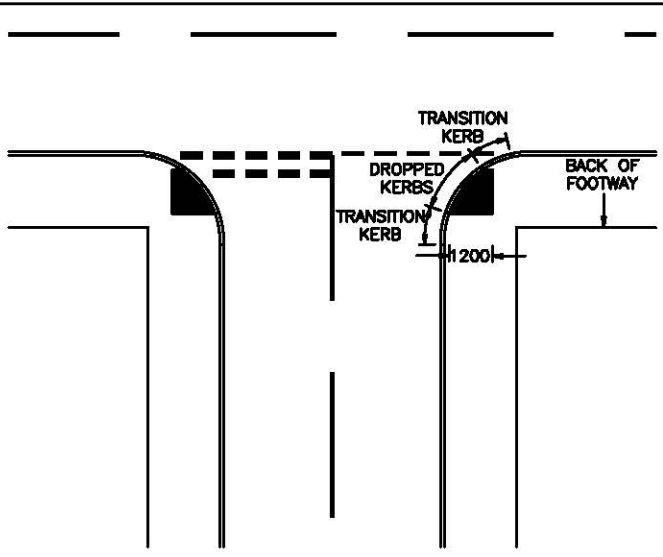
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 KERBS, FOOTWAYS
 PEDESTRIAN REFUGE LAYOUT

REV.	DESCRIPTION	CHECKED	DATE

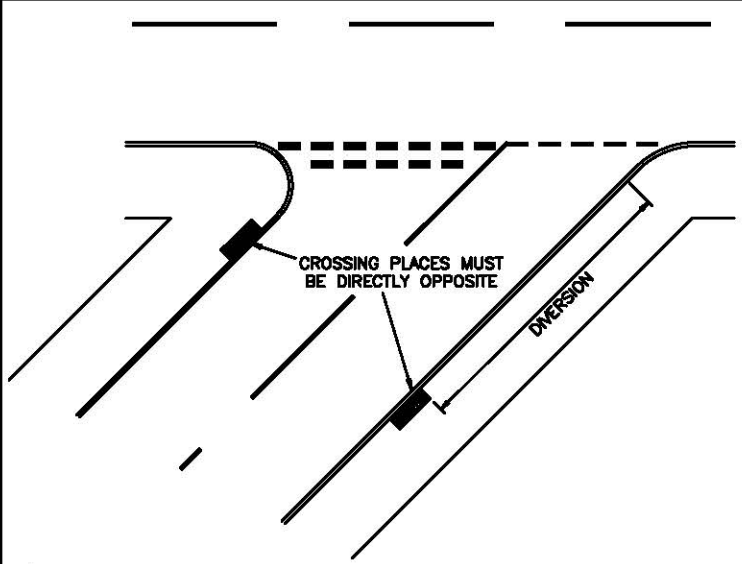
	INIT.	DATE	PROJECT TITLE	
SURVEYED BY			DEVELOPER STREET SCAPE DETAIL	
DESIGNED BY			SCALE	FILE No.
DRAWN BY			N.T.S	APRIL 2015
CHECKED BY			DRAWING No.	DSD-1100-10



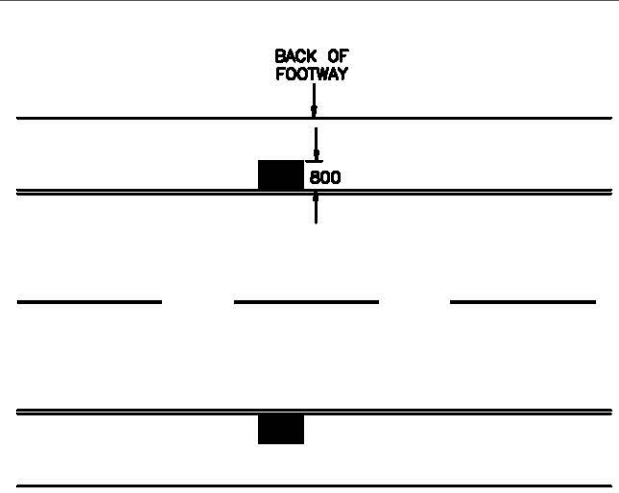
A) INDENTED UNCONTROLLED CROSSING POINT AT A SIDE ROAD



B) IN-LINE UNCONTROLLED CROSSING POINT



C) INDENTED UNCONTROLLED CROSSING AT ACUTE ANGLED JUNCTION



D) UNCONTROLLED CROSSING AWAY FROM JUNCTION

NOTES:

1. ALL DIMENSIONS IN MILLIMETRES.
2. TACTILE SLABS TO BE LAID PARALLEL TO THE CROSSING.
3. TACTILE SLABS TO BE COLOURED RED AT SIGNAL CONTROLLED CROSSINGS AND BUFF COLOURED AT OTHER CROSSINGS.
4. FOR FURTHER DETAILS OF LAYOUTS, REFER TO DETR DOCUMENT: 'GUIDANCE ON THE USE OF TACTILE PAVING SURFACES'

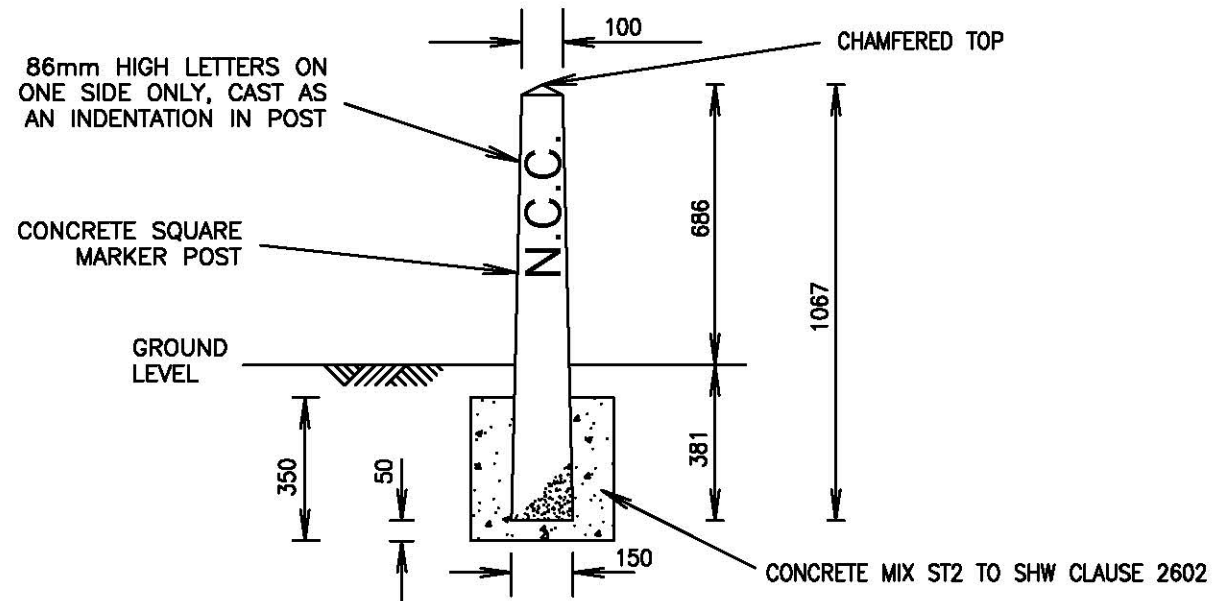


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DRAWING TITLE
 KERBS, FOOTWAYS
 PEDESTRIAN AND CYCLE CROSSING POINTS
 TYPICAL LAYOUTS

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		DEVELOPER STREET SCAPE DETAIL	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	APRIL 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1100-11	



NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. REINFORCEMENT TO CONSIST OF 4 No. 9mm DIA. MILD STEEL BARS WITH 3mm BINDING WIRE SPIRALLY WOUND AT 75mm PITCH.
3. PRE-CAST UNITS WEIGHING IN EXCESS OF 20kgs ARE TO BE PLACED BY MECHANICAL MEANS.
4. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.

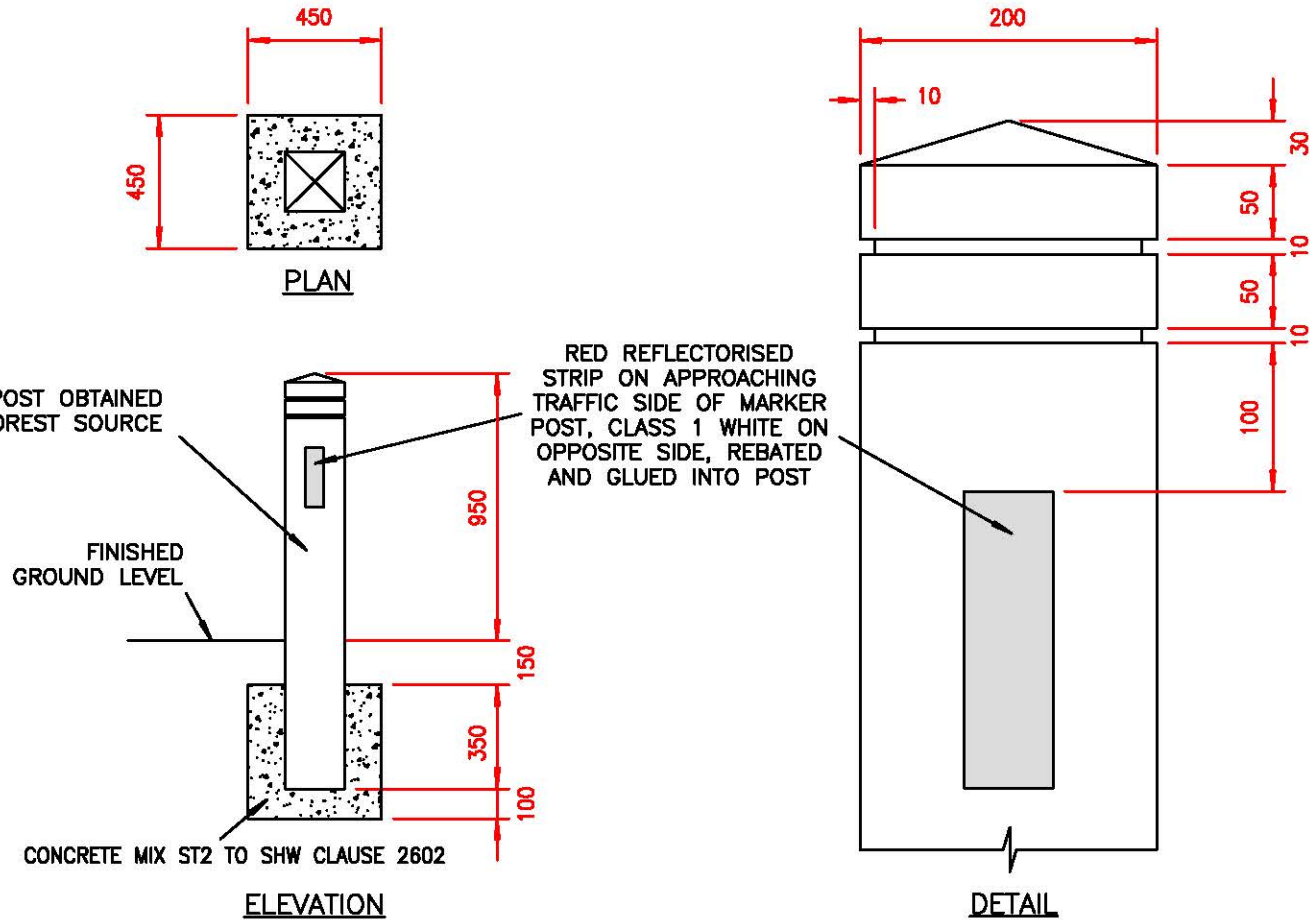


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DRAWING TITLE
 TRAFFIC SIGNS AND ROAD MARKINGS
 NCC BOUNDARY MARKER POST

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		STANDARD CONSTRUCTION DETAILS	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	April 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1200-01	



NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.

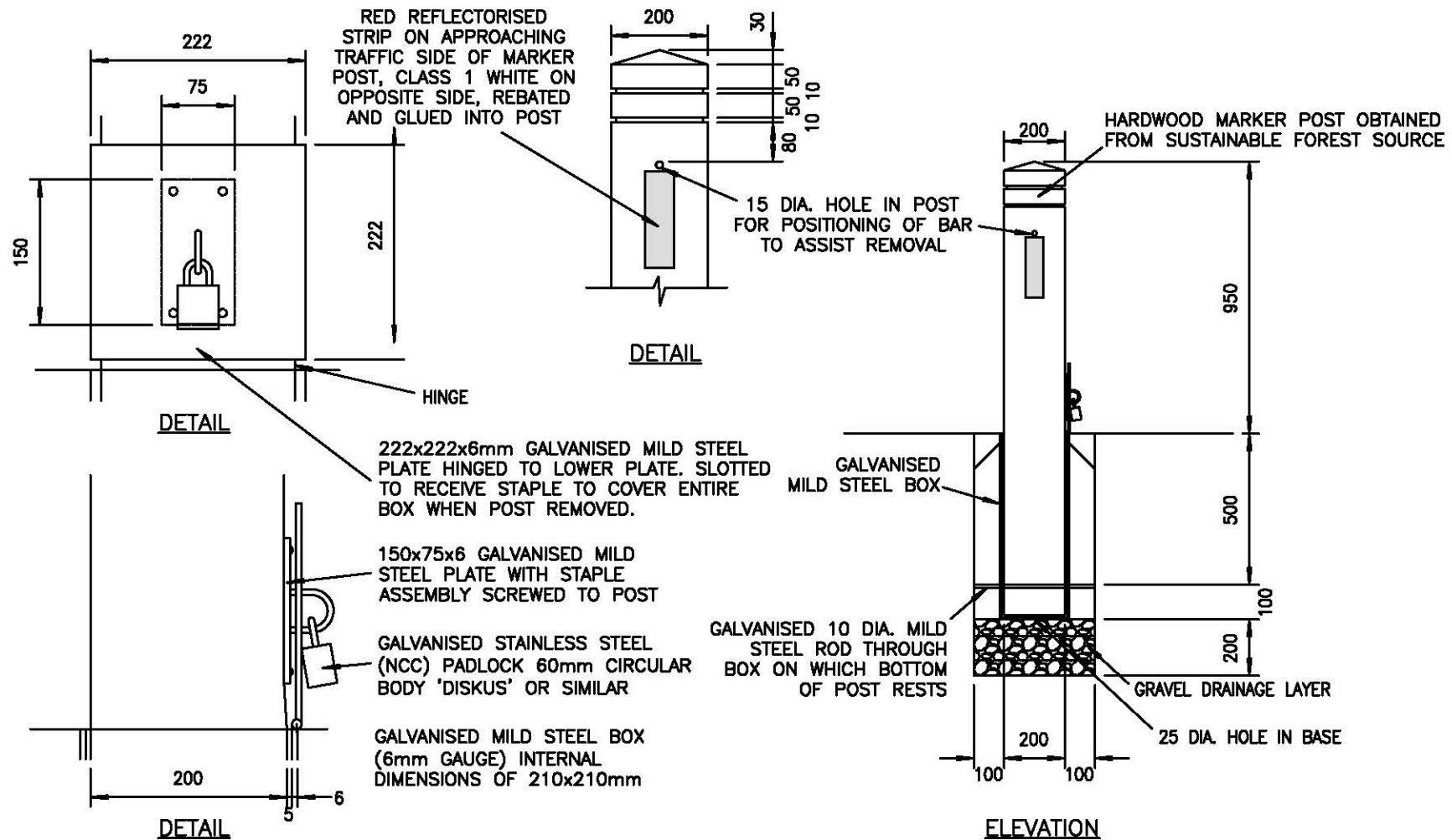


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DRAWING TITLE
 TRAFFIC SIGNS AND ROAD MARKINGS
 TIMBER BOLLARD

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		STANDARD CONSTRUCTION DETAILS	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	April 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1200-02	



NOTES:

1. ALL DIMENSIONS IN MILLIMETRES UNLESS OTHERWISE STATED.
2. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.

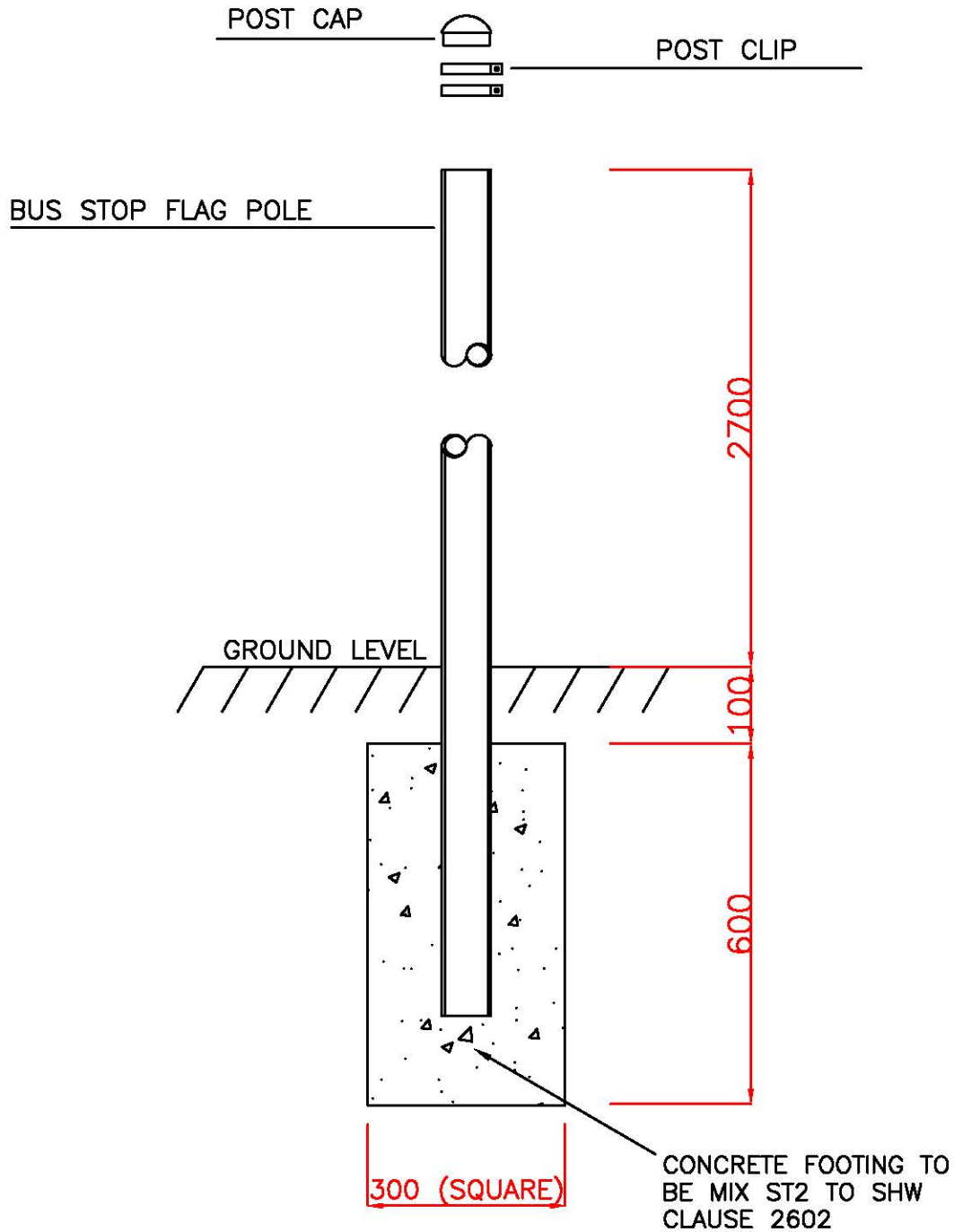


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DRAWING TITLE
 TRAFFIC SIGNS AND ROAD MARKINGS
 TIMBER BOLLARD (REMOVABLE)

REV.	DESCRIPTION	CHECKED	DATE

INIT.	DATE	PROJECT TITLE	
		STANDARD CONSTRUCTION DETAILS	
SURVEYED BY		SCALE	DATE
DESIGNED BY		N.T.S	April 2015
DRAWN BY		DRAWING No.	
CHECKED BY		DSD-1200-03	



NOTES:

1. POLE TO BE 76.1 X3.2 WALL THICKNESS. FINISH TO BE GALVANIZED EXCEPT IN CONSERVATION AREA BLACK PLASTIC COATING TO BE USED.
2. ALL DIMENSIONS IN MILLIMETERS UNLESS OTHERWISE STATED
3. ALL SAFETY MEASURES ARE TO BE TAKEN WITH REGARD TO BREAKING OUT AND MECHANICAL MEANS ARE TO BE USED WHERE POSSIBLE. IF BREAKING-OUT GUNS ARE USED, THE USE OF ANTI-VIBRATION GLOVES AND ROTATION OF LABOUR IS RECOMMENDED.



DRAWING TITLE

TRAFFIC SIGNS AND ROAD MARKINGS
BUS STOP FLAG POST AND MOUNTING DETAILS

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Norwich NR1 28G

REV.	DESCRIPTION	CHECKED	DATE

SURVEYED BY	INIT.	DATE	PROJECT TITLE	
			DEVELOPER STREET SCAPE DETAIL	
DESIGNED BY			SCALE	DATE
DRAWN BY			NTS	April 2015
CHECKED BY			DRAWING No. DSD-1200-04	